



Marine Users Working Group

Presented By: Fraser Crossing Partners and
Transportation Investment Corporation

October 30, 2025

Welcome

- Review of September 25 Meeting Minutes
- Navigation Log
- Project Update
- River Monitoring Update
- Demolition Planning Update
- 4 Week Look-Ahead Schedule
- Next Steps

Review of Meeting Minutes

September 25, 2025

Meeting Minutes – Sept. 25, 2025

- Navigation log
 - No marine incidents to report during the period.
- FCP provided a Project update:
 - Progress of cantilever construction continues, with activities focused on SM-20. Bridge finishing works and cable stay installation ongoing.
 - Construction of the main span towards New West is nearly complete.
 - Underslung platforms and a gantry reducing clearance by 3.65 m will support bridge finishing works; NAVWARNS will be issued to alert mariners.
 - The new bridge is scheduled to open by Christmas.
- FCP provided a River Monitoring Update:
 - September 2025, river monitoring were presented and compared with previous months. Established thresholds continue to be monitored.
 - The impact of removing all trusses and infrastructure on the river's bathymetry will be fully understood post-demolition activities.
- FCP provided a Demolition Planning Update:
 - Demolition will proceed in stages: deck removal, steel trusses, and in-river piers above and below water. Blasting at piers 2–4 will include a bubble curtain system, 450 m traffic safety zone, and occur during slack tide (weekends preferred).
 - A 90-minute TSOP will be in place with air horn signals; marine, rail, pedestrian, and vehicular coordination will involve CN Rail, VFPA, and police.
 - AV warnings will go out 96 hours in advance, followed by daily notices; MUWG/IMUWG will be notified 4 weeks prior.
 - The demolition communications plan has been approved by the Port; a separate public communications sub-plan is in development.
- FCP provided a 4-week look-ahead:
 - Construction work on SM 19 started on September 12 and is expected to last two to three weeks, with no interference to navigation.
 - The next segment, SM 20, will start on September 29, also with a duration of two to three weeks, and no navigation interference.

Navigation Log

Marine Occurrences as a Result of the Project

**No updates to report*

Project Update

Works in Progress

Project Update

S1 Tower and Cantilever Construction



Photo 1: Bridge deck asphalt, cable stay, fence installation and other finishing works in progress



Photo 2: Main span and North Approach bridge connection works ongoing

Project Update

South Approach Bridge Deck Concrete/Asphalt Overlay



Photo 3: South Approach-end span- main span cable stay installation and finishing works ongoing



Photo 4: South Approach-end span and main span concrete overlay, finishings and paving ongoing

Project Update

N1 Tower and North Approach



Photo 5: North Approach deck panels installation and deck pours ongoing

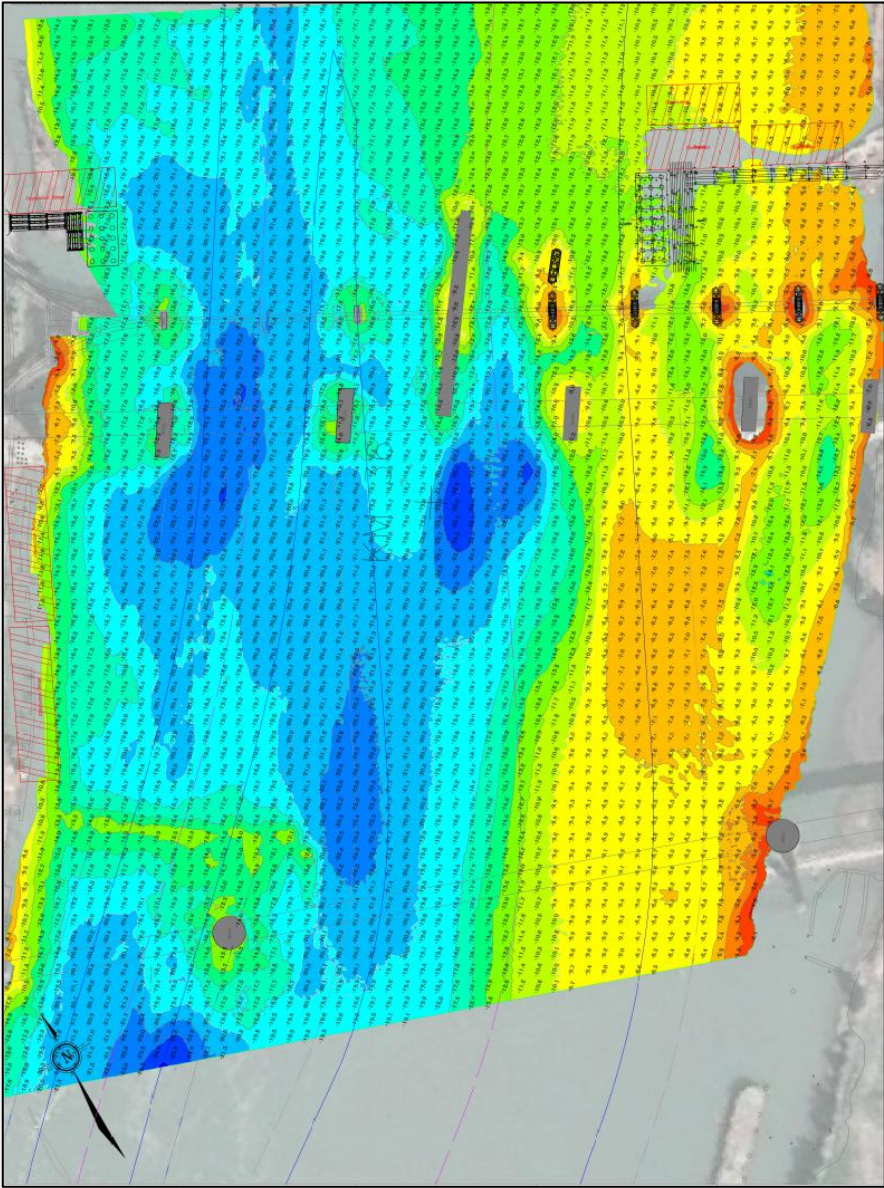


Photo 6: Main span almost connected to the North Approach

River Monitoring Update

Monitoring Results

Bathymetry Monitoring Survey Results – October 1, 2025

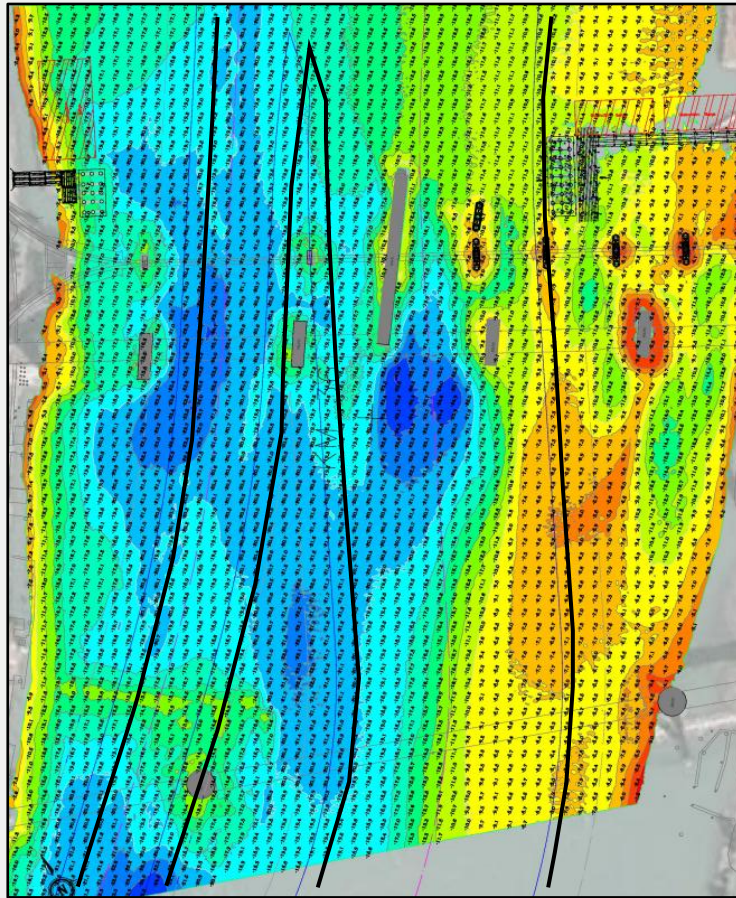


Elevations Table			
Number	Maximum Elevation	Minimum Elevation	Color
14	0.00	-2.00	Red
13	-2.00	-4.00	Orange
12	-4.00	-6.00	Yellow
11	-6.00	-8.00	Light Green
10	-8.00	-10.00	Green
9	-10.00	-12.00	Light Blue
8	-12.00	-14.00	Blue
7	-14.00	-16.00	Dark Blue
6	-16.00	-18.00	Very Dark Blue
5	-18.00	-20.00	Black
4	-20.00	-22.00	Dark Grey
3	-22.00	-24.00	Medium Grey
2	-24.00	-26.00	Light Grey
1	-26.00	-28.00	White

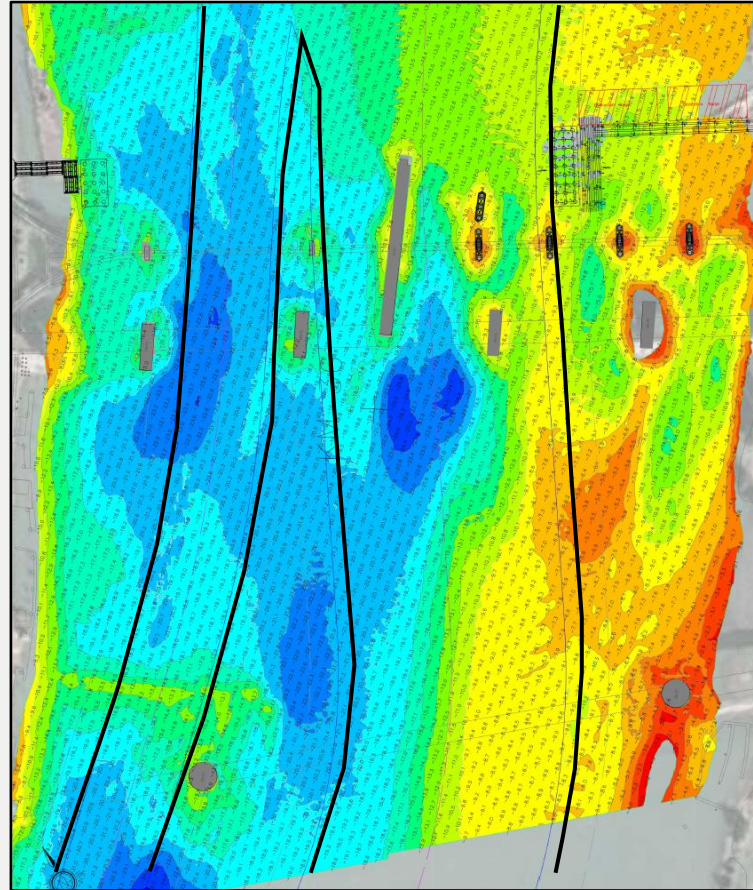
Bathymetry Results:

Comparison of Survey Results - Sep 2024 to Oct 2025

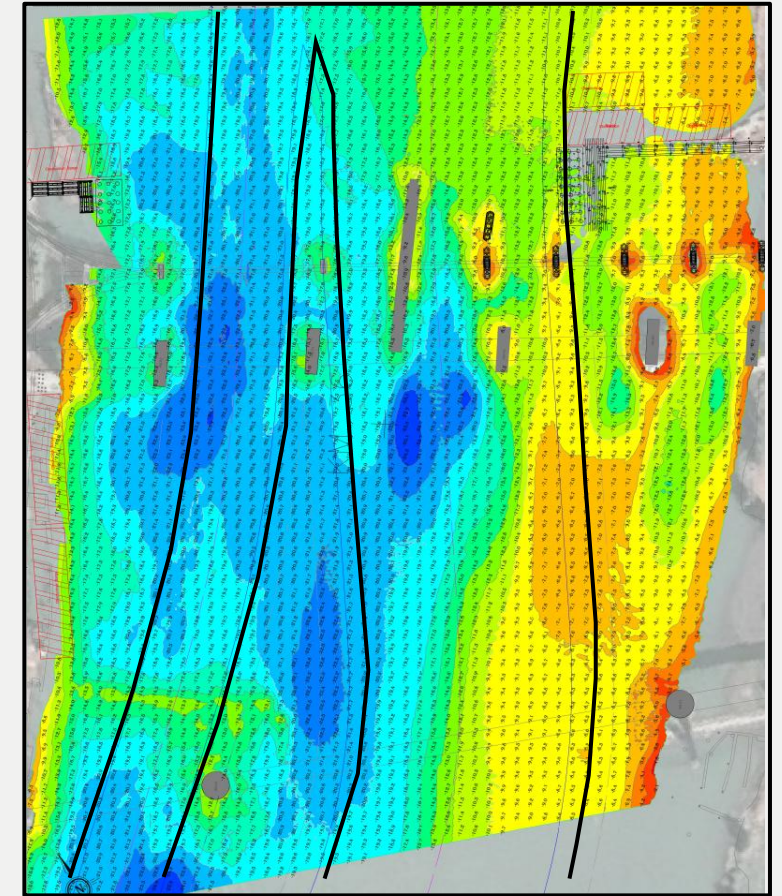
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September 23, 2024



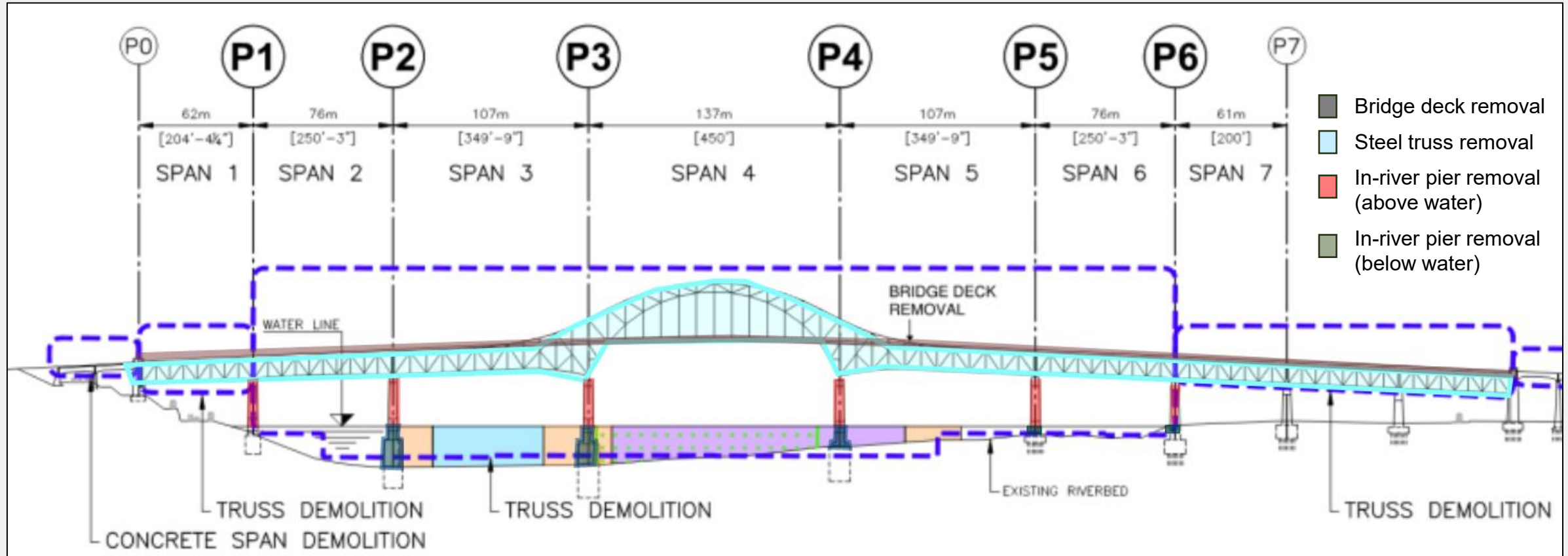
February 10, 2025



October 1, 2025

Demolition Planning

Demolition Plan

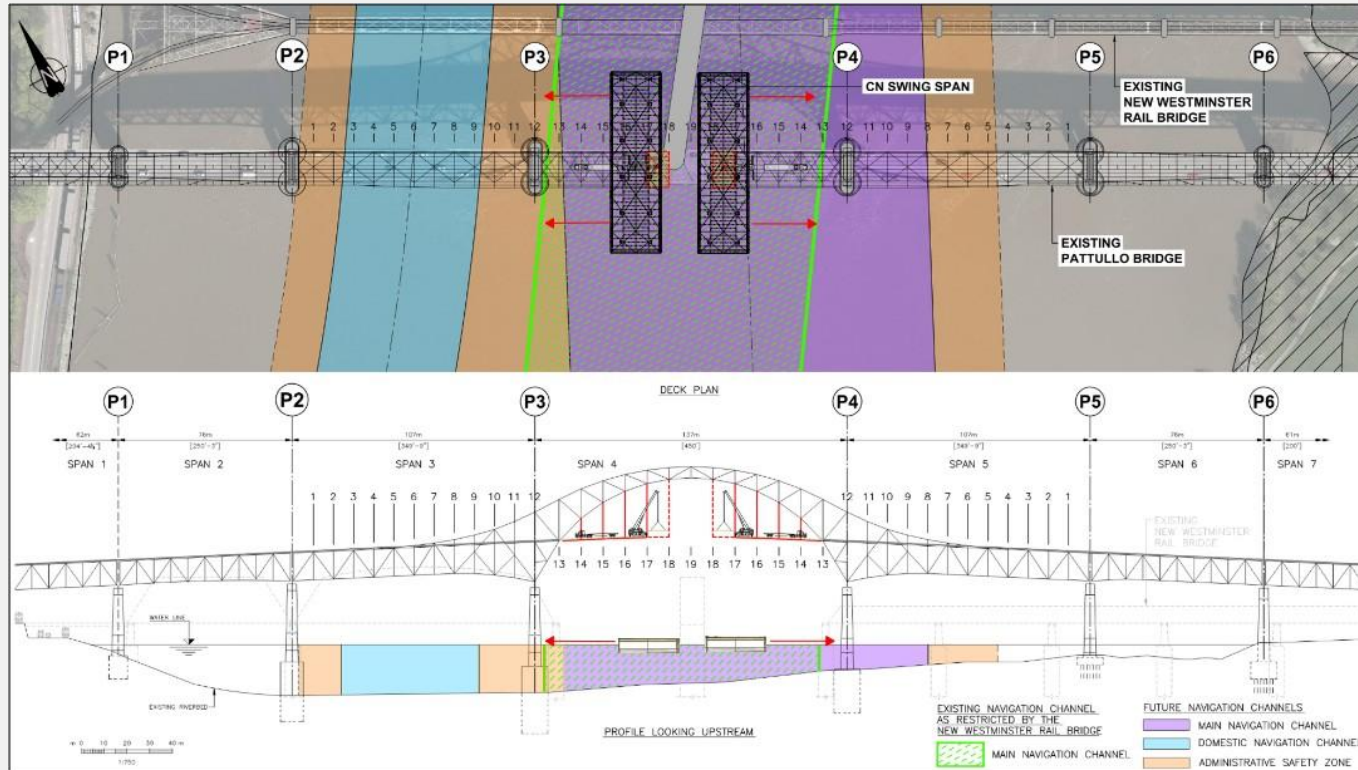


Demolition Planning – Updates Shared

Bridge deck removal	Steel truss removal	In-river pier removal (above water)	In-river pier removal (below water)
<ul style="list-style-type: none"> Methods remain the same (as per Marine Demolition Communication Plan Rev2 and Marine Demolition and Stage Plan Rev2 approved in Aug/Sep 2024) 	<ul style="list-style-type: none"> Steel truss (Span 4) removal via strand jacks and lowering span onto barge(s) (Jan 30/25 IMUWG/MUWG) Similar lowering approach on remaining in-river spans (May 29/25 IMUWG/MUWG) Updated methods limits the number of cuts and reduces interferences to navigation 	<ul style="list-style-type: none"> In-river pier removal above water via conventional demolition (P1 to P6) (Mar 27/25 IMUWG/MUWG) Equipment positioned on flexi-float modular barge which surrounds the pier and contains the materials Updated methods reduces duration of works 	<ul style="list-style-type: none"> Technical Evaluation of Removal Methods In-river pier removal below water via conventional demolition (P1, P5, P6) and controlled blasting (P2, P3, P4) (Jul 31/25 IMUWG/MUWG) Sequencing of activities associated with controlled blasting, safety measures and proposed marine communications shared (Sep 25/25 IMUWG/MUWG)

To support initial works proposed for starting January 2026 (i.e., bridge deck and steel truss removal), marine plans have been updated to capture these 2 phases of works. Remaining demolition activities (i.e., in-river pier removal) will be included in a future update to the plans.

Bridge Deck Removal



Summary of Deck Removal:

- Two crews working “outward” from the center
- Sawcut asphalt/concrete bridge deck into pieces and lift on a flat bed truck
- Cut and remove steel girders and cross braces
- Removed material taken to “drop points” on either end of the bridge
- Removed material will be taken to an offsite recycling facility

Mitigation Measures:

- Implementation of BMPs:
 - Management and containment of materials on the bridge roadway (e.g., vacuum/hydrovac removal of slurry)
 - Positioning barge equipment below specific activities (e.g., saw cutting) to capture potential falling debris.

Implications to Navigation:

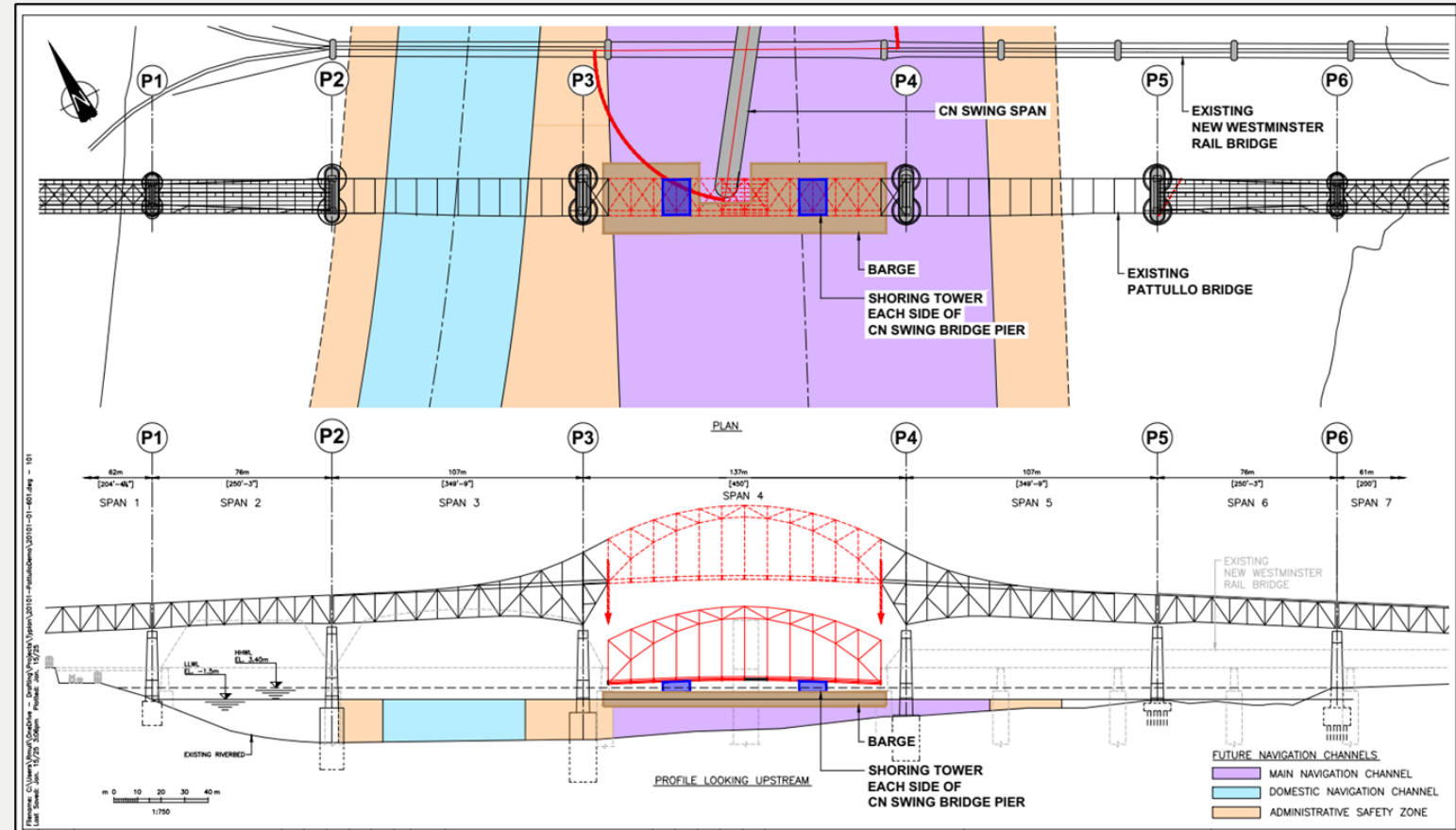
- Partial restrictions to main and domestic channels based on barge positioning
- Estimated duration of works = 10 wks (Jan-Apr 2026)

Steel Truss Removal

Span 4

Summary of Truss Removal:

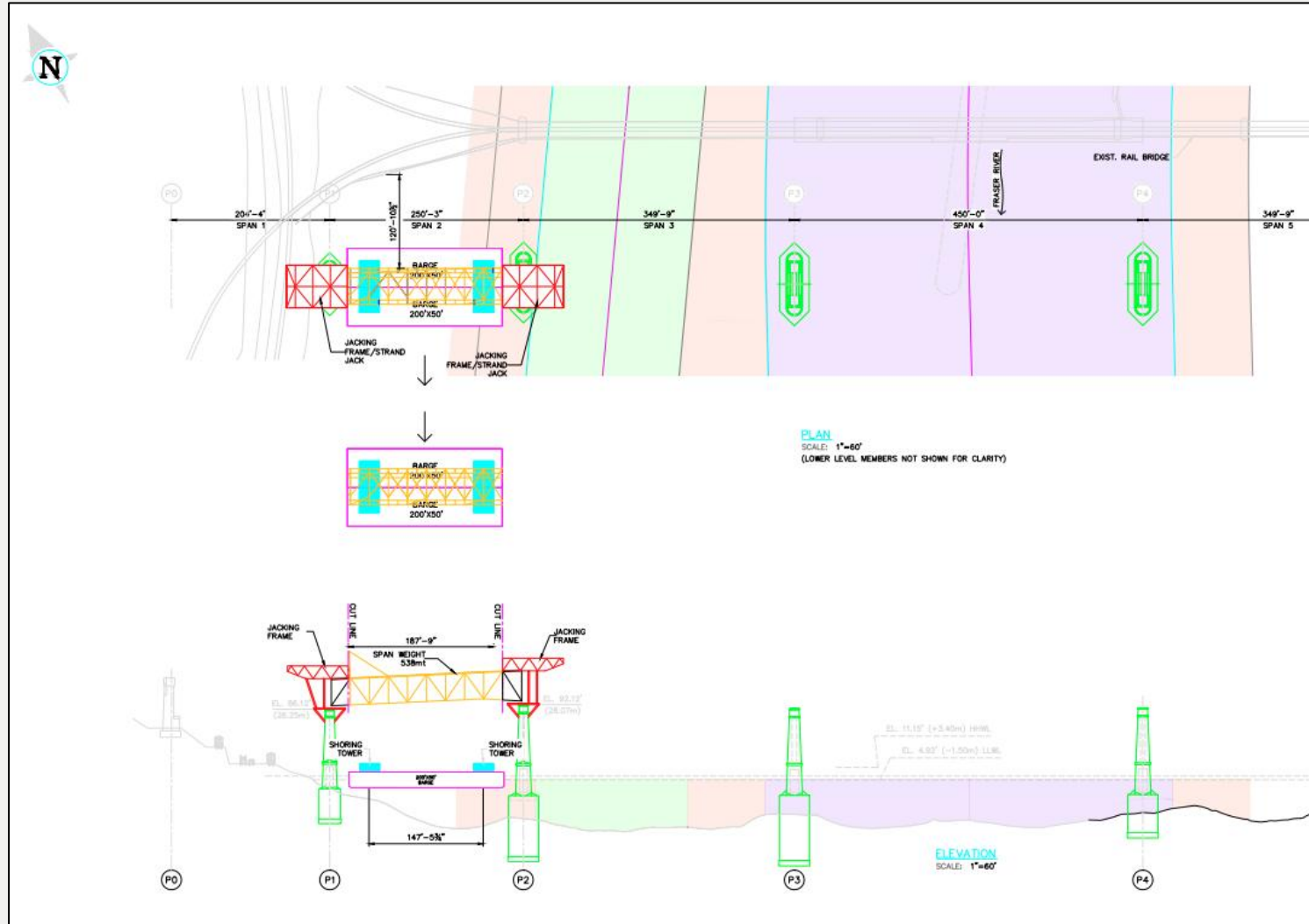
- Set up barge next to piers with shoring towers positioned on either side of a vertical member
- Use strand jacks to lift up the span off the pier(s) bearings
- Move barges with steel span away from piers (up to 8 weeks for preparation works)
- Lower steel span down onto barges (up to 3 days)
- Send directly to recycling facility



Implications to Navigation: Requires short term closure of main navigation channel (12-36 hrs); Feb-Apr 2026

Steel Truss Removal

Example: Span 2



Summary of Truss Removal (spans 2, 3, 5, 6):

- Install temporary struts and jacking frames, engage the strand jacks and cut the span
- Set up barge next to piers with shoring towers positioned on either side of a vertical member
- Using strand jacks, lower the span and place on barge
- Move barge with span away from pier

Implications to Navigation:

- Equipment sited outside of the domestic navigation channel; no restrictions to navigation
- Estimated duration of works: 4 wks (Feb-Sep 2026)

Marine and Fisheries Access Management Plan (MFAMP)

Updates

- Section 2: Purpose and Scope
 - **Added Table 1-1: Table of Concordance with EAC Condition 2 and 13** (similar to CEMP & DEMP per EAO's request)
 - **Added Section 2.1.1: Plan Update Process** (same text as included in CEMP & DEMP per EAO's request)
- Section 3: Description of Project Components *added demolition components*
 - **Bridge deck removal** – description of works and schedule
 - **Steel truss removal** – description of works and schedule
- Section 7: Potential Effects to Marine and Fisheries Access *added demolition components*
 - **Bridge deck removal** – restrictions to navigation
 - **Steel truss removal** – restrictions to navigation
- Section 8.1.2/8.1.3: Marine Construction and Staging Plan/Marine Communication Plan
 - *Added* reference to **Marine Demolition and Staging Plan/Marine Demolition Communication Plan**

VFPA Marine Plan Updates

Marine Demolition and Staging Plan

- Section 1.1: Purpose and Scope
 - **Added Table 1: Table of Concordance with VFPA PER Permit Conditions**
- Section 5: Demolition Works Potentially Interfering with Navigation
 - **Phase 1: Bridge Deck Removal** - updated exhibits
 - **Phase 2: Superstructure (Steel Truss) Removal** - updated exhibits, proposed schedule and potential interferences to navigation based on refinements to the methods

Marine Demolition Communication Plan

- Section 1.1: Purpose and Scope
 - **Added Table 1: Table of Concordance with VFPA PER Permit Conditions**
- Section 3: Demolition Overview and Schedule
 - **Phase 1: Bridge Deck Removal** - updated exhibits
 - **Phase 2: Superstructure (Steel Truss) Removal** - updated exhibits, proposed schedule and potential interferences to navigation based on refinements to the methods

Fisheries Access

Anticipated Demolition Schedule*

- Demolition methods/refinements intended to minimize in-water work durations.
- Periods of least risk to key fish species were identified based on the relative monthly abundance; generally, were less likely to be present from Nov – Feb of any given year.
- Critical months for First Nation fisheries were considered (i.e., avoiding July and August).

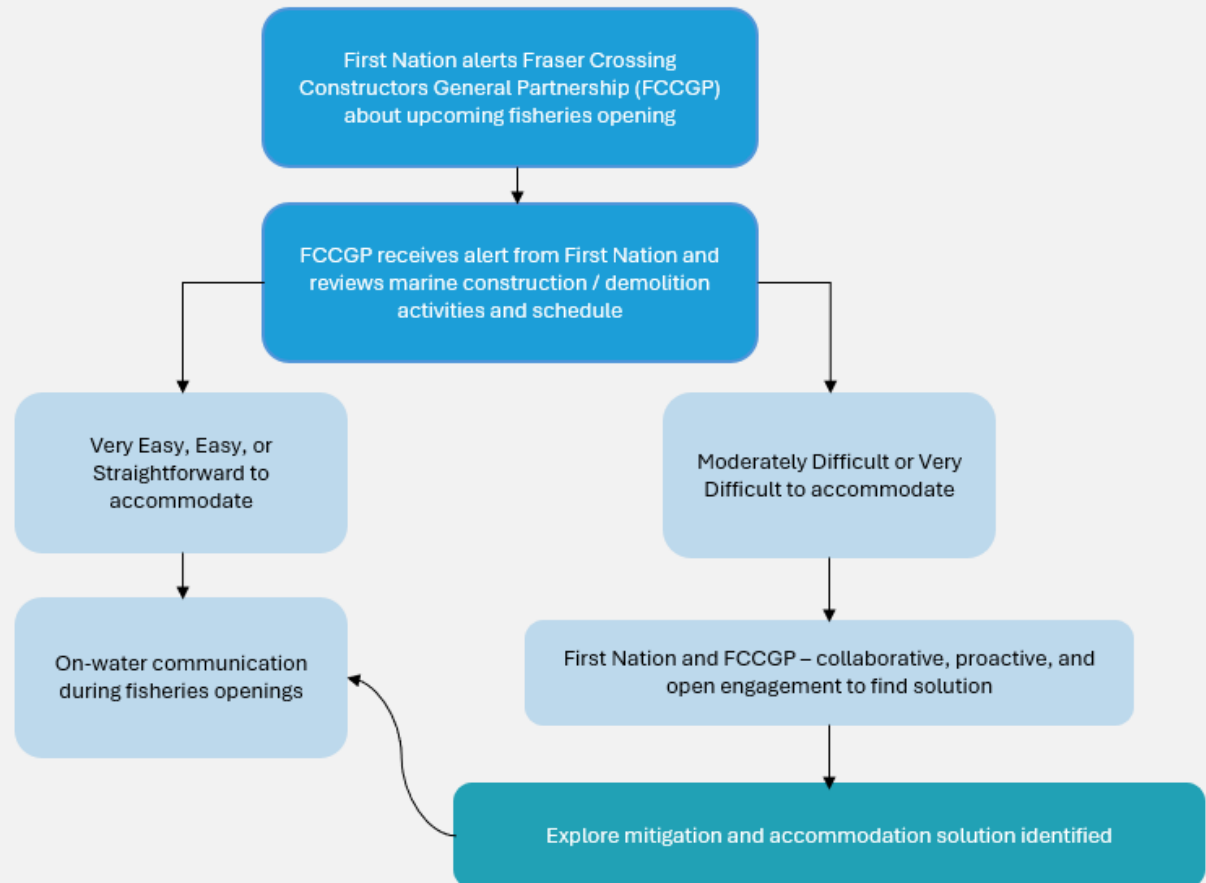
Activity	Location	Timeframe
Bridge deck removal	Main span, working from centre outwards	Jan – Mar 2026
Superstructure removal	Spans P1-P6	Feb – Sep 2026
In-river pier removal (above)	Piers P1-P6	Aug – Oct 2026
In-river pier removal (below water)	Piers P1, P5, P6 (conventional)	Nov 2026 – Jan 2027
	Piers P2, P3, P4 (controlled blasting)	Oct 2026 – Jan 2027

*Note: *Schedule is subject to First Nation consultation and required approvals and authorizations (e.g., Fisheries Act Authorization)*

Fisheries Access

Indigenous Marine Users Two-Way Communication Protocols

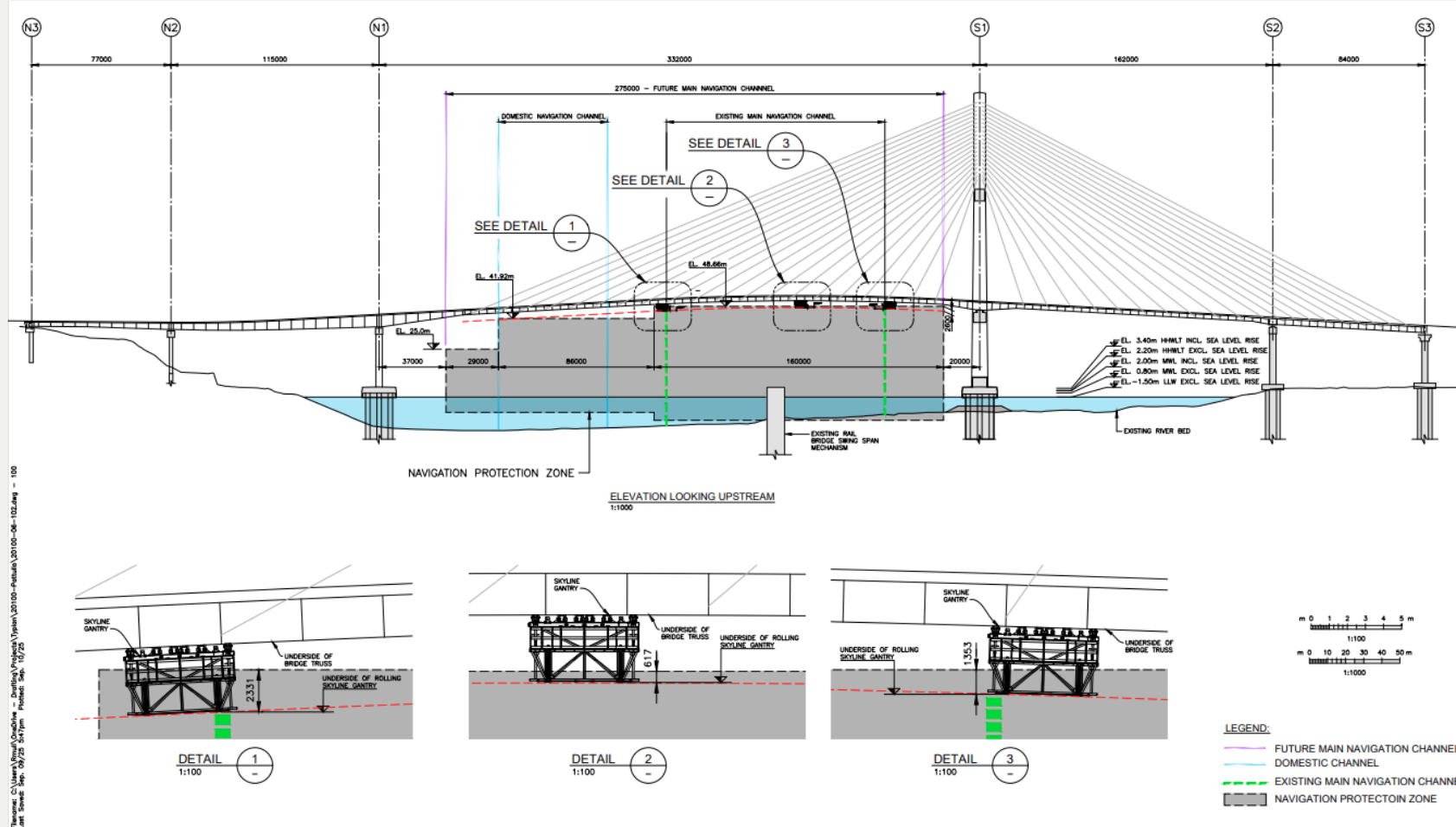
- Project has committed to avoid or minimize disruptions to access to fisheries (Section 8.1.3 - Marine and Fisheries Access Management Plan).
- Key activities identified as potentially interacting with fisheries access include in-river pier removal; these works are scheduled outside of critical fishing months of July and August.
- For other potential interactions of demolition activities with fisheries, coordination between FCCGP and First Nations will be accomplished through Two-Way Communication Protocols.
- Protocols include key contacts and communication processes for adjusting demolition activities to accommodate access.



4-Week Look-Ahead / Construction Staging

Construction Staging – Main Bridge Construction

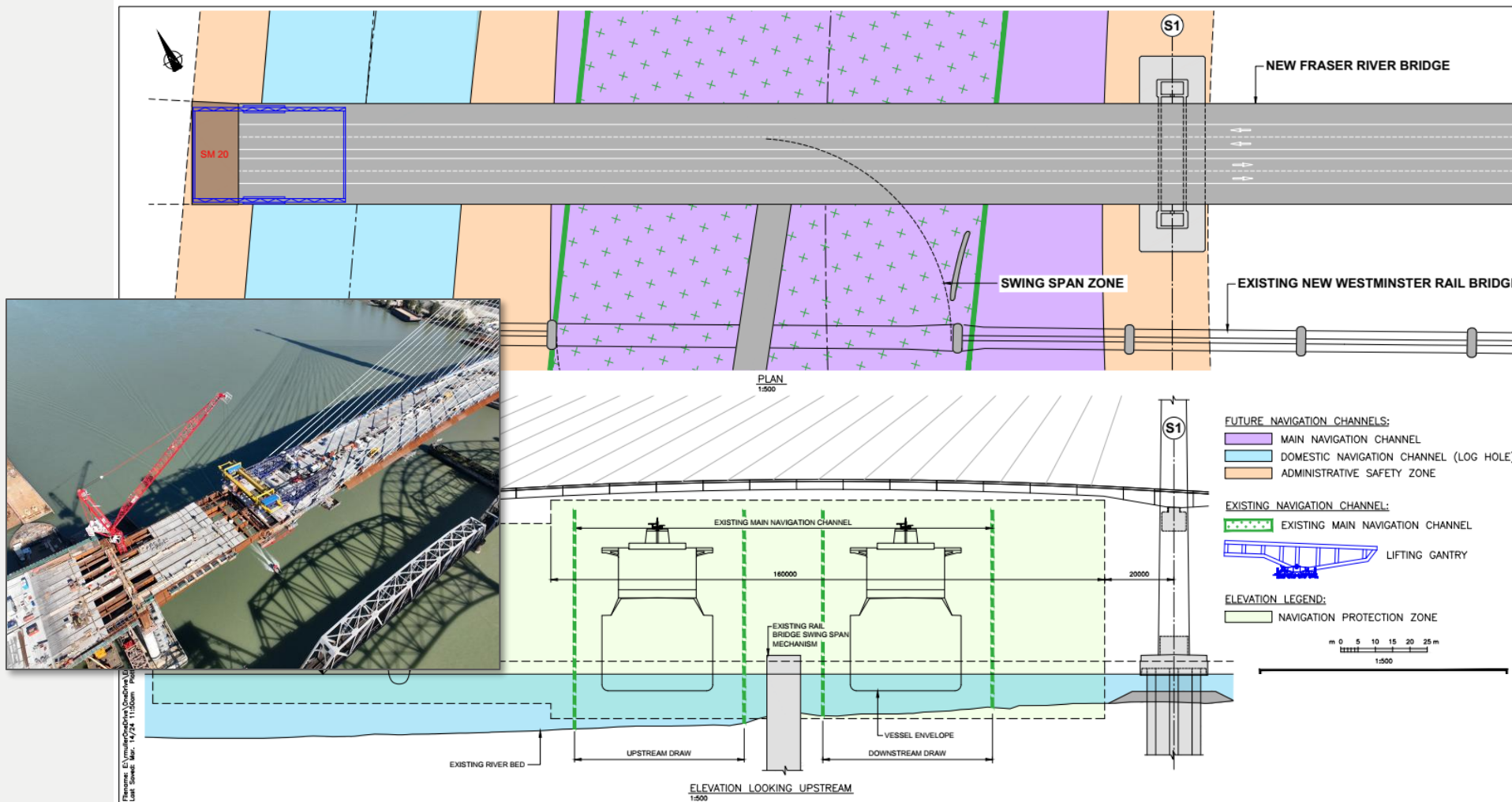
Underslung Platform and Gantry



- Finishing works continuing to end of 2025
- Gantries partially intrude (vertically) into the navigation protection zone, reducing the available clearance to mariners by 3.65 m.
- Regular NAVWARNs will be issued indicating their transitions over the main navigation channel.

Construction Staging – Cantilever Construction

Segment SM-020 Connection to North Approach



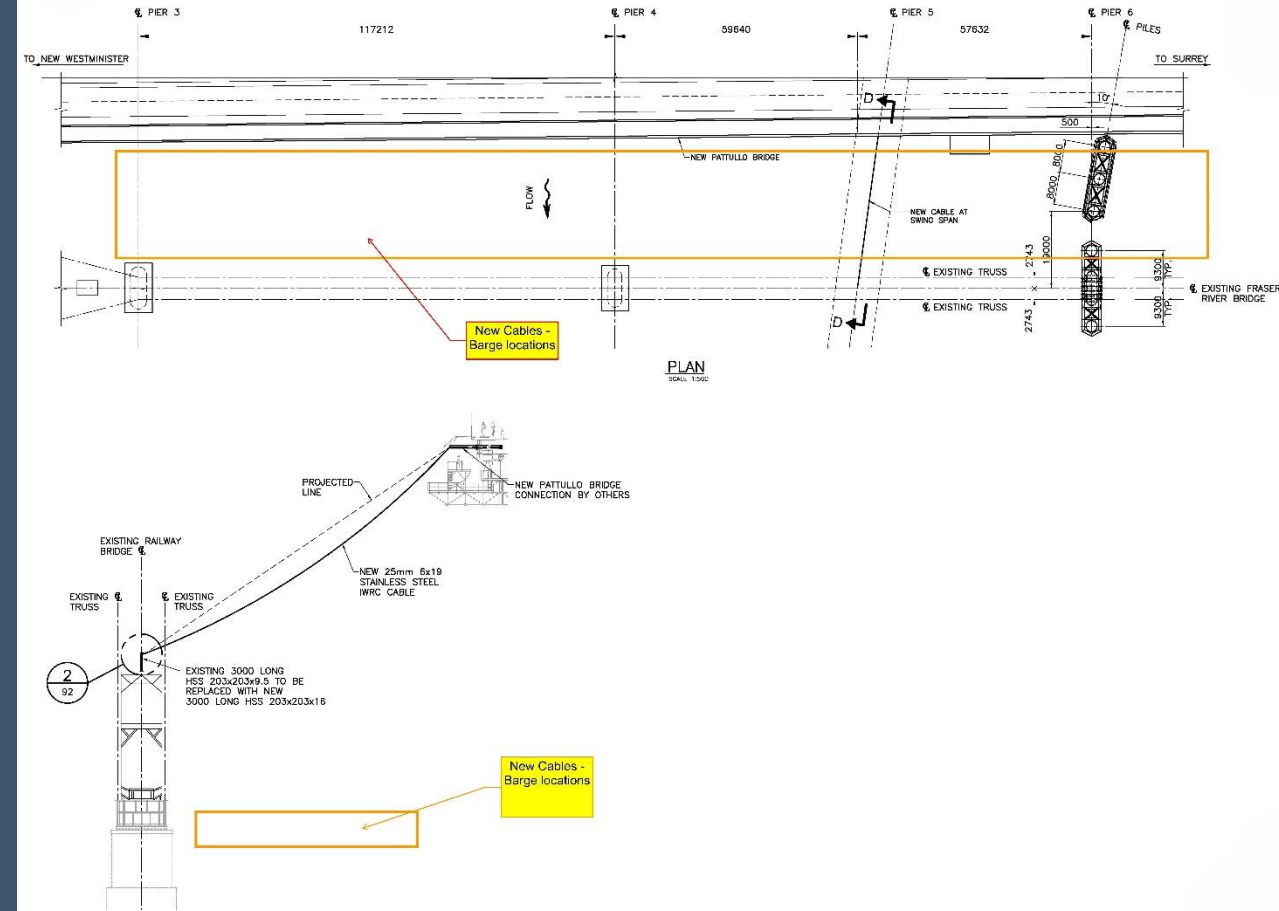
- Connection of SM-020 with North Approach mid November
- 1-2 days
- No interference to navigation

CN Fraser River Bridge - Cables Relocation

Cables run FRB signals, communication and bridge power

Construction Timelines,

- Installation of new cables: Dec. 2025 – Feb. 2026
- Removal of existing cables: Feb. 2025 – Mar. 2026

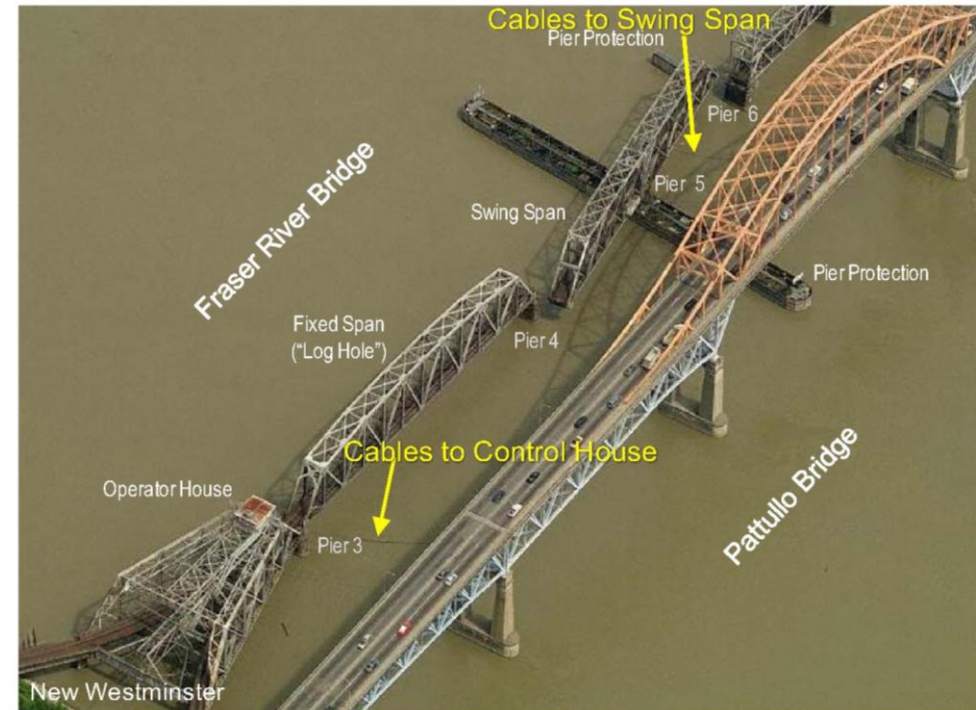
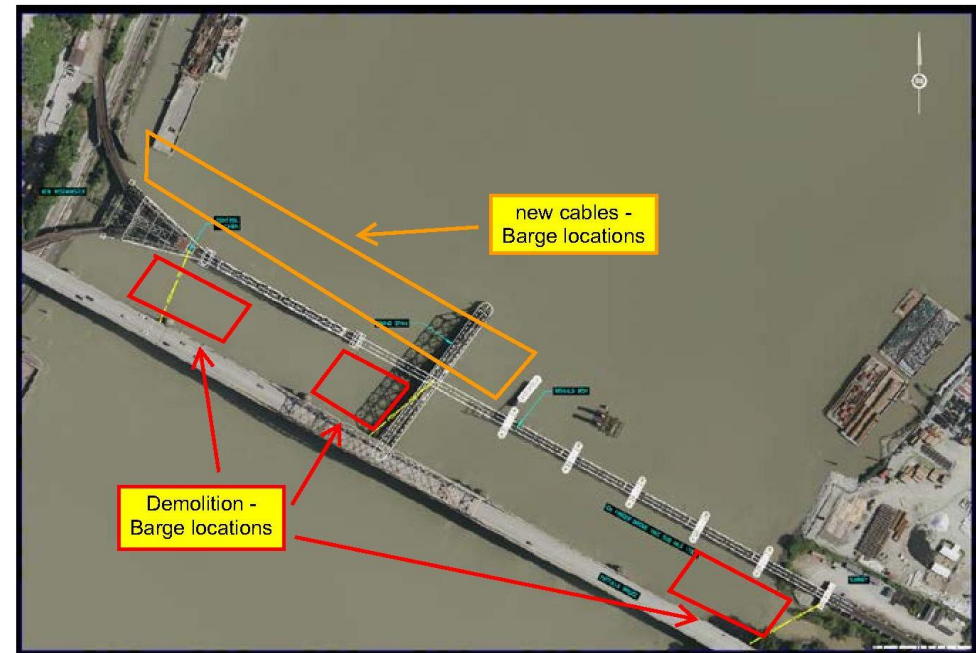


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Cables run FRB signals, communication and bridge power

Construction Timelines,

- Installation of new cables: Dec. 2025 – Feb. 2026
- Removal of existing cables: Feb. 2025 – Mar. 2026



Next Steps

Ongoing NAVWARNs to be provided

Next Marine Users Working Group:

- November 27, 2025 (TBC)